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Appendix G
Parking Survey Report

National Transport Authority
**Blanchardstown to City Centre
Core Bus Corridor Scheme**
Parking Survey Report

Issue | 27 April 2022

This report takes into account the particular instructions and requirements of our client.

It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.

Job number 268401-00

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Appendices

Appendix A

Existing On-Street Parking Spaces on Blanchardstown to City Centre Preliminary Parking Study Area (Ashtown Road to Arran Quay)

Appendix B

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1 Introduction

1.1 Background

The BusConnects programme aims to transform Dublin's bus system, so that journeys by bus will be fast, reliable, punctual, convenient, affordable, and with greater scope for interconnection between routes. A key aspect of the BusConnects proposal is the provision of major infrastructure enhancements for buses, cyclists and pedestrians along each of the main bus corridors. Provision of high-quality footpaths, cycle tracks and bus lanes involve some local road widening, but also necessarily requires reallocation of roadspace where corridor width is heavily constrained by adjacent buildings. In some cases, reallocation of roadspace involves removal of on-street parking bays. This report investigates the impact on local parking activity and supply associated with the Blanchardstown to City Centre Core Bus Corridor Scheme (hereafter referred to as 'the Proposed Scheme'). The impact on loading / unloading activities is also assessed.

Parking duration surveys were originally planned to assist in parking investigations, however, the COVID-19 outbreak and associated restrictions on movement meant that any surveys carried out during this period would not reflect typical parking patterns. A desktop study was therefore carried out to assess the impact of the BusConnects infrastructure proposals on parking. The study is based on inspection of current parking supply and compares the existing parking supply to the parking supply with the Proposed Scheme in place.

The purpose of this Parking Survey Report is hence to:

- Quantify the current and proposed on-street parking bays;
- Identify the current parking regulations;
- Establish the dominant local land uses and expected parking characteristics.

The report contains separate analyses of a number of 'self-contained' corridor sections, where groups of parking bays can be considered to provide a local parking supply. The change in on-street parking supply has been identified and assessed in the context of the local needs and adjacent land uses.

The local adjacent parking supply and characteristics have also been noted where relevant. For the Proposed Scheme, the self-contained sections, where changes are proposed to parking supply, are listed below and are illustrated in **Figure 1**.

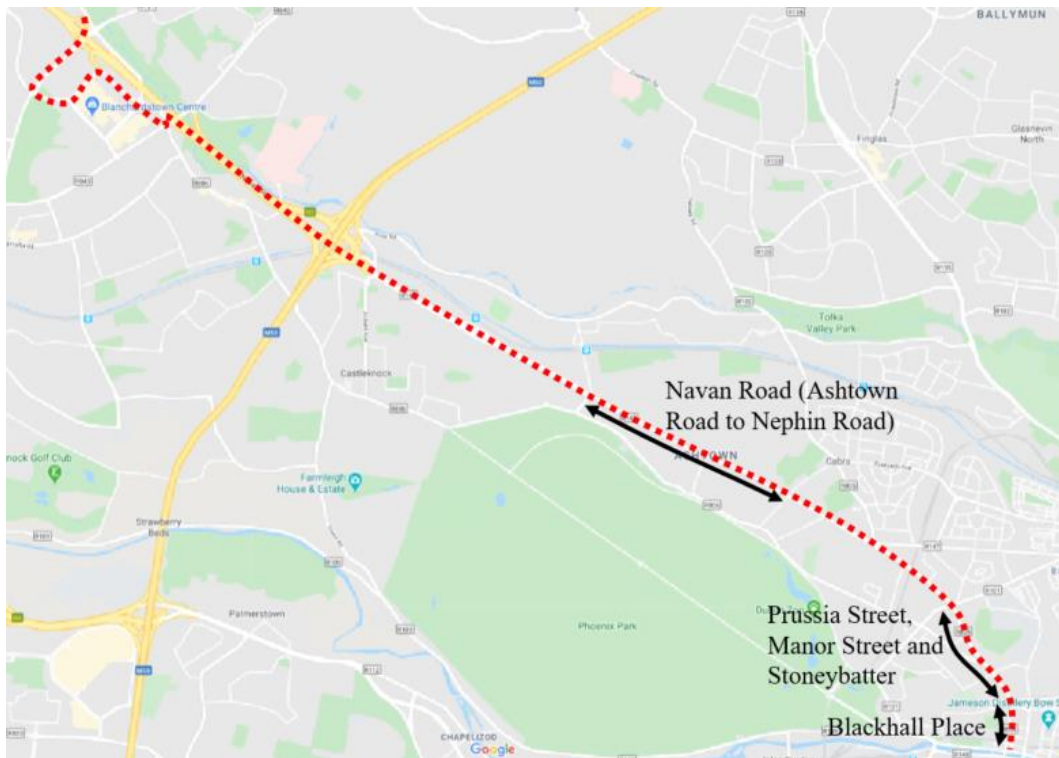


Figure 1: Blanchardstown to City Centre – Sections with Parking Changes

- Navan Road (between Ashtown Road and Nephin Road);
- Prussia Street, Manor Street and Stoneybatter (between North Circular Road and Brunswick Street North); and
- Blackhall Place, Queen Street, Brunswick Street North, King Street North and Blackhall Street (between Brunswick Street North and Ellis Quay).

It should be noted also that construction of the proposed Blanchardstown Bus Interchange at the northern end of the scheme involves removal of approximately 130 parking spaces in the northern car park of the shopping centre. There are around 1800 spaces at present in the northern car park, and nearly 6000 spaces overall at the shopping centre. It is considered that reducing the parking supply by approximately 130 spaces (to make space for the bus interchange) will not have a significant impact on parking capacity - on the basis that a small increase in bus mode share (due to the BusConnects scheme) from car to bus by shoppers and employees would offset the reduction in car spaces. This off-road parking impact

has therefore not been included in this parking study of the impact on on-street parking provision.

1.2 Assumptions and Approach

The approach adopted to quantifying parking impacts is based on utilising the existing topographical surveys, mapping and site visits in order to determine the scale of existing parking availability.

The existing parking availability has then been compared against the proposed quantity of parking as set out in the General Arrangement drawings for the Proposed Scheme and tabulated to present the change in quantum of parking arising as a result of the Proposed Scheme.

Key assumptions in the assessment are:

- That the existing parking regulations at each group of parking spaces will continue to apply to parking spaces provided at or close to the same location in the Proposed Scheme; and
- All parking alterations located within the red line boundary of the Proposed Scheme are considered to form part of the corridor parking quantum.

It should be noted that this report does not include consideration of cycle parking; it should also be noted the operation of bus lanes is for a lesser period than 24 hours per day (say 0700 – 1900, or 0700 – 2100) and may be utilised as parking outside of the hours of operation, which has not been considered in this study. Clearways also exist along the Proposed Scheme, and may be utilised as parking outside of the hours of operation which has not been considered in this study.

2 Methodology

2.1 Introduction

The approach adopted in this study involves identifying both the baseline parking and loading bay situation and the future provision of parking and loading bays with the Proposed Scheme infrastructure in place and comparing the two scenarios in respect of the number of spaces available. A description of the existing land use and parking supply in the vicinity of the Proposed Scheme is also presented.

2.2 Baseline Parking and Loading

In order to understand the baseline parking situation along the Proposed Scheme, the following information was collated via desktop analysis (making use of local authority data on parking regulations, using 'streetview' images from online resources and site visits):

- On-street parking regulations and spaces along the Proposed Scheme;
- Location of time-limited bus lanes / cycle lanes which allow parking during unregulated periods;
- Loading bays;
- On-street parking regulations and spaces on side streets (limited to those streets which connect directly with the proposed Scheme and only counting spaces within 200m of the Proposed Scheme).

The existing parking regulations for each group of parking bays were classified as follows:

- Designated Paid Parking;
- Designated Permit Parking;
- Disabled Permit Parking;
- Designated Loading Bays;
- Designated Taxi Ranks; and
- Informal / Unregulated Parking (free parking).

For both the existing and future parking supply, it has been assumed that parking space needed for each car is 6m in length.

It is noted that certain locations have 'mixed' regulations; in particular many on-street parking bays are shared between paid parking and resident permit parking (and in order to quantify the change in parking on these shared bays, it has been assumed that usage is split equally between both types of parking). Quantifying informal parking on unregulated locations is, of necessity, based on desktop analysis and local knowledge. Illegal parking has been noted where it appears to occur on a regular basis but has not been included in the baseline parking supply.

Land uses on and surrounding the Proposed Scheme corridor have also been reviewed in order to fully understand local parking characteristics, and off-street parking provision noted where relevant.

2.3 Future Parking and Loading

The future on-street parking supply with the Proposed Scheme in place has been identified from the final scheme drawings. For the purposes of this report, it has been assumed that the existing parking regulations at each group of parking spaces will continue to apply to parking spaces provided at or close to the same location in the Proposed Scheme.

3 Parking Impact on Navan Road (between Ashtown Road and Nephin Road)

3.1 Baseline Parking and Loading Analysis

3.1.1 Corridor On-Street Parking Bays and Regulation

Navan Road, between Ashtown Road and Nephin Road, is a single carriageway with a bus lane in the southbound direction from 07.00 to 19.00 (Monday to Saturday) and one lane per direction for general traffic. A section of the outbound direction has a bus lane from Kempton Avenue which continues to the west of Navan Road. The location and provision of current spaces (or bays) on Navan Road (between Ashtown Road and Nephin Road) are illustrated in **Appendix A** and summarised in **Table 1**.

There are kerbside on-street parallel parking bays of approximately 5 spaces outside of The Brophy Medical Practice (near the Parish Church), which are available outside the no-parking hours of between 07.00 and 10.00 (Monday to Saturday). There is also a long continuous layby, interspersed with driveways, which provides approximately 19 spaces on the outbound side of Navan Road (opposite Maxol service station).

The rest of this section has no on-street parking available, with a majority of residential properties in this section having private parking within their premises. It should be noted that site observations indicates that some illegal parking occurs on the footpath.

Table 1: Existing On-Street Parking Spaces on Navan Road

Sub-section	Existing Parking Facilities	Number of Spaces
Navan Road (between Ashtown Road and Nephin Road)	Parking on north side within a continuous layby with intermittent driveways	19 spaces (approximately)
Navan Road (between Ashtown Road and Nephin Road)	Parking on north side in layby outside The Brophy Medical Practice	5 spaces

3.1.2 Loading Bays

There are no on-street loading bays between Ashtown Road and Nephin Road. It can be assumed that loading activities occur within adjacent premises or outside bus lane regulation and Clearway hours.

3.1.3 On-Street Parking Bays and Regulation on Local Side Streets

There are no locally accessible on-street parking spaces on local side streets directly off Navan Road between Ashtown Road and Nephin Road.

3.1.4 Land Use

This section of the Proposed Scheme on Navan Road between Ashtown Road and Nephin Road has predominantly residential houses with private parking within their premises, as shown in **Appendix B**.

The main trip attractors in this section are St Vincent's Special National School, St Vincent's Centre, Aura De Paul Swimming Pool, St John Bosco Junior Boys' School, Our Lady of Help Christians Parish Church, Maxol fuel station and The Halfway House (public house). A majority of these trip attractors have parking available within their premises; however, the two schools generate parking activities in the morning and afternoon during school drop-off/pickup. In addition, it is understood that some events at the Parish Church result in parking overspill on the unregulated spaces in front of houses on the south side of Navan Road (where there is space for approximately 19 cars) – although there is parking space within the Church car park.

3.2 Proposed Scheme Parking Proposals

3.2.1 Proposed On-Street Parking and Loading Bay Supply

With the Proposed Scheme in place, there is an associated need to remove some parking spaces to provide improved facilities for pedestrians, cyclists, and buses. The planned change in on-street parking on Navan Road between Ashtown Road and Nephin Road is illustrated in **Figure 2**, and shown in **Table 2**.

Table 2: Existing and Proposed Parking Supply Summary (Navan Road)

Sub-section	Parking Facilities	Existing	Proposed	Loss of Parking
Navan Road (between Ashtown Road and Nephin Road)	Parking (informal)	24 spaces, made up of 19 spaces on the south side of the Navan Road and 5 spaces on the north side of the Navan Road.	4 spaces on north side of Navan Road	19 spaces removed from south side of Navan Road. 1 space from the north side of the Navan Road

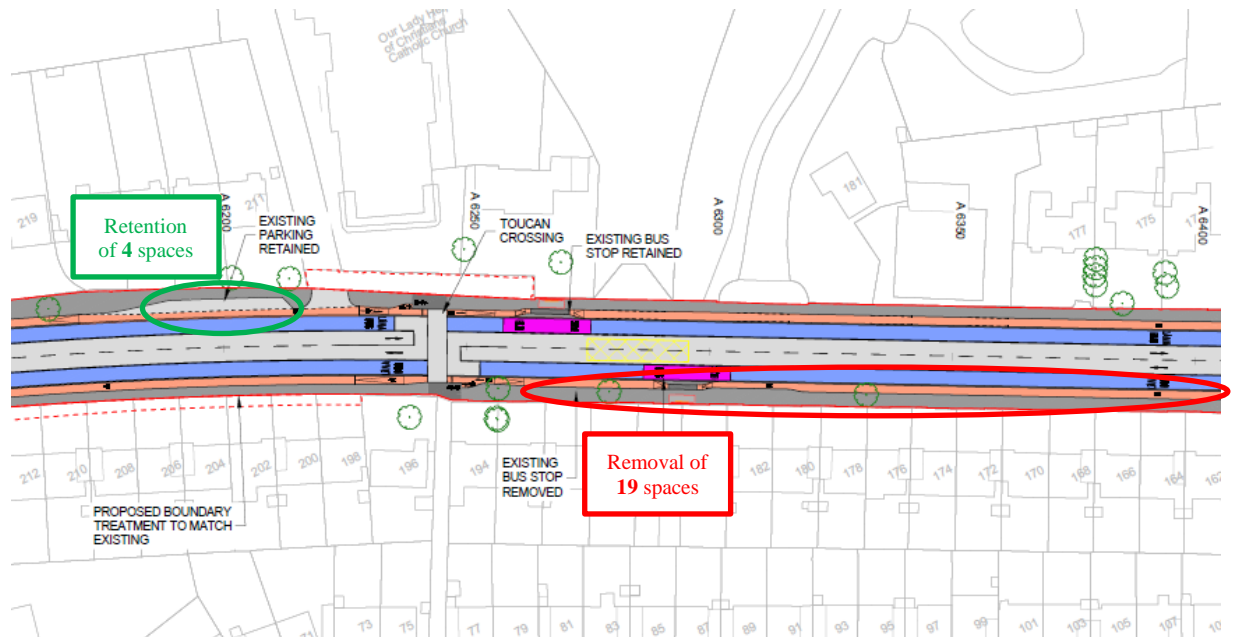


Figure 2: Proposed scheme design on Navan Road (west of Nephin Road)

3.2.2 Loading Bays

There are no on-street loading bays between Ashtown Road and Nephin Road, and no change is proposed.

3.3 Overall Parking Impact

A summary of the parking impact with respect to the change in overall parking supply on the Navan Road section of the Proposed Scheme (between Ashtown Road and Nephin Road) is shown in **Table 3**, which includes consideration of spaces on adjacent streets within an approximate 200m distance.

Table 3: Impact of Parking Changes for Navan Road

Location	Type of Parking		No. of Spaces		
			Baseline	Scheme	Change
Between Nephin Road and Baggot Road	Informal	Residential	19	0	-19
Between Nephin Road and Baggot Road	Informal	General / Residential	5	4	-1
Approx. adjacent on-street parking within 200m			24	24	0
Totals			48	28	-20

4 Parking Impact on Prussia Street, Manor Street, Stoneybatter and Aughrim Street

4.1 Baseline Parking and Loading Analysis

4.1.1 Corridor On-Street Parking Bays and Regulation

The section of the Proposed Scheme on Prussia Street, Manor Street, Stoneybatter and Aughrim Street (between Northern Circular Road and Brunswick Street North) generally has Pay and Display and Permit Parking available, particularly towards the city centre (i.e. between North Circular Road and Ellis Quay). These parking spaces can be used by residents with parking permits and businesses/visitors are able to pay for parking at the ticket machines or online. This section is within the Dublin Parking Tariff High Demand Red Zone, which has a rate of €2.70 per hour. The maximum allowable parking time at Pay & Display areas is three hours and vehicles are not permitted to return to the same street until one hour has elapsed.

The location and provision of current spaces (or bays) on Prussia Street, Manor Street, Stoneybatter and Aughrim Street are illustrated in **Appendix A**.

The current parking supply is summarised as follows (and shown in **Table 4**):

- On Prussia Street there is a short length of kerbside parking (for Pay & Display and Permit Parking from 10.00 to 19.00 Monday to Saturday), with sufficient space for approximately 10 cars; there are 23 permits allocated to this street;
- On Manor Street and Stoneybatter there is kerbside parking with space for approximately 58 cars (including 2 disabled parking bays), available for Pay & Display and Permit Parking from 07.00 to 19.00 Monday to Saturday; there are 54 permits allocated to this street;
- On Aughrim Street at its junction with Manor Street, there is kerbside parking available for up to 7 cars outside Kavanagh's Pub, available for Pay & Display and Permit Parking from 07.00 to 24.00 Monday to Sunday; there are 53 permits allocated to Aughrim Street.
- On Aughrim Street at its junction with Manor Street, there is kerbside parking available for up to 4 cars in the northbound lane, available for Pay & Display and Permit Parking from 07.00 to 19.00 Monday to Saturday; and
- There are 4 separate loading bays, 3 of which are on Manor Street and Stoneybatter, and another on the southern end of Aughrim Street at the junction with Manor Street.

It is noted that there is also space along the kerb for parking on Prussia Street, Manor Street and Stoneybatter outside Clearway hours which is in place between 07.00 and 10.00 and 12.00 and 19.00 (Monday to Saturday). However, this has not been included in the analysis as it is only available for two hours during the

day and therefore would have minimal impact on the overall parking supply during the daytime.

Table 4: Existing On-street Parking and Loading Spaces at Prussia Street, Aughrim Street, Manor Street, Stoneybatter and Manor Place

Sub-section	Existing Parking Facilities	Number of Spaces
Prussia Street	Designated Paid Parking and Permit Parking	10 spaces
Manor Place	Designated Paid Parking and Permit Parking	1 space
Manor Street / Stoneybatter	Designated Paid Parking and Permit Parking	58 spaces
	Disabled Permit Parking	2 spaces
	Loading/Unloading in designated loading bays	3 bays (4 spaces)
Aughrim Street / Manor Street Junction	Designated Paid Parking and Permit Parking	11 spaces on Prussia St / Manor St / Aughrim St junction
	Loading/Unloading in designated loading bays	1 bay (2 spaces) immediately west Manor St / Aughrim St junction

4.1.2 On-Street Parking Bays and Regulation on Local Side Streets

There are a number of side streets which are able to be used by local residents and visitors / businesses throughout the corridor section. These spaces are likely to be utilised by some residents and visitors to premises on Prussia Street, Manor Street and Aughrim Street. A summary is set out below:

- On St Joseph’s Road (off Prussia Street) there is space for approximately 35 parking bays, and one disabled parking bay at Pay & Display and Permit Parking spaces (30 permits have been allocated to this street), from 07.00 to 19.00 Monday to Saturday. The ‘Church of the Holy Family’ on Aughrim Street is a trip attractor in this location. These spaces are also likely to be utilised by residents and visitors to premises on Prussia Street, as an alternative to parking on Prussia Street, where the Clearway regulation effectively means that overnight parking is inconvenient, as cars would need to be moved by 07.00;
- On Manor Place, west of Manor Street there is Pay & Display and Permit Parking (48 permits have been allocated to this street) from 07.00 to 19.00 Monday to Saturday– with approximately 56 bays; these are able to presently

serve as an overspill area for parking by residents and visitors on Prussia Street; and

- On Kirwan Street, east of Manor Street there are approximately 33 spaces for Pay & Display and Permit Parking (34 permits are allocated to this street) from 07.00 to 19.00 Monday to Saturday. Similar to Manor Place, this area is able to presently serve as an overspill area for parking by residents and visitors on Prussia Street.

These side streets add approximately 124 parking spaces to the overall parking supply – within easy walking distance of Prussia Street / Manor Street. From observation, these spaces are relatively busy but with some spaces available during the day (on a typical weekday).

4.1.3 Land Use

Prussia Street, Manor Street and Stoneybatter are surrounded by a mixture of land uses which have different parking behaviours and distribution and is shown in **Appendix B**.

To the east, Park Shopping Centre is the biggest trip attractor in this section, which has Tesco and other smaller businesses including takeaways, a hairdresser, offices, a gymnasium and a post office. These generally have a high parking turnover particularly in the afternoon peak / after work hours. Park Shopping Centre has approximately 116 parking spaces and therefore would generally not be expected to cause parking overspill onto Prussia Street.

The western side of the Proposed Scheme corridor along Prussia Street is predominantly residential with small shops and industrial uses. Vehicle parking can take place on local side streets for parking during 'Clearway' hours (i.e. 07.00 to 10.00 and 12.00 to 19.00) on the corridor. There is a commercial / industrial area to the west of the corridor on Prussia Street which has off-street parking for approximately 20 vehicles. Additionally, a steel fabrication yard is located on the western side of Prussia Street - with customer parking on its premises and hence with a negligible impact on on-street parking.

The Proposed Scheme is also in close proximity to the children's playground and playing fields on the Grangegorman site, which is a large urban development site in Dublin. There is however no direct access to the Grangegorman site and hence there is negligible impact on on-street parking activity along the corridor.

The western side of the corridor at Manor Street is predominantly residential properties with intermittent pockets of retail / restaurant / takeaway / public house land use. The owners and visitors to residential properties on Manor Street have access to on-street Pay & Display and Permit Parking on Manor Street, as well as to local side streets (including Manor Place and Kirwan Street). Parking on these side streets is potentially more available during weekdays as some residents will have vacated on-street spaces to drive to work or to go shopping etc.

The eastern side of the Proposed Scheme (between Kirwan St and Brunswick Street North) has a mixture of retail, residential, restaurants, takeaways and public

houses, and hence parking activity is expected to be relatively constant throughout the day and evening.

Educational facilities are also located off Manor Street immediately east of the corridor (Stanhope Street Primary School and Stanhope Street Girls Secondary School). Therefore, the trip peaks here would be expected in the morning and afternoon during school drop-off/pickup.

Community facilities are also located immediately east of the corridor, which have direct access to Grangegorman Lower (parallel to Manor Street) and therefore do not require on-street parking along the corridor.

There is also a large site, south of the community facilities, which is Dublin City Council's Mechanical Division and also includes a Dublin Fire Brigade depot. Trip patterns in this area would typically be evenly spread throughout the day.

4.1.4 Loading Bays

Loading activities will generally take place intermittently throughout the day – and hence loading bays should be 'empty' most of the time. Observations suggest that shoppers are using the loading bays to park and visit local shops.

There are three loading bays on Manor Street / Stoneybatter: two on the west side, and one on the east side. The likely users of the eastern loading bay are Centra, as well as the numerous public houses/bars/restaurants/takeaways on Manor Street, such as The Glimmer Man, L Mulligan Grocer, Tommy O'Gara's and Shangri-la. It should be noted that this is a long loading bay that could cater for up to 3 small vans.

On the western side, the two single loading bays would be primarily used by shops that are located within close proximity such as Manor DIY, Mahon's Butchers, Pender's Stoves & Fireplaces and The Green Grocer.

4.2 Proposed Scheme Parking Proposals

4.2.1 Proposed On-Street Parking and Loading Bay Supply

With the Proposed Scheme infrastructure in place there is an associated need to remove some parking spaces to provide improved facilities for pedestrians, cyclists, and buses – which inevitably requires some re-allocation of on-street parking space. This section describes the planned provision of on-street parking supply.

Overall, it is anticipated that better laid out on-street parking within a kerb-protected bay arrangement, together with continued use of on-street parking on local side roads, will provide a robust and adequate parking provision into the future – when considering also the much improved accessibility by bus and cycle modes.

Prussia Street

On Prussia Street, the short length of kerbside parking (for Pay & Display and Permit Parking from 10:00 to 19:00 Monday to Saturday) will be removed (which has space for around 10 cars), as shown in **Figure 3**.

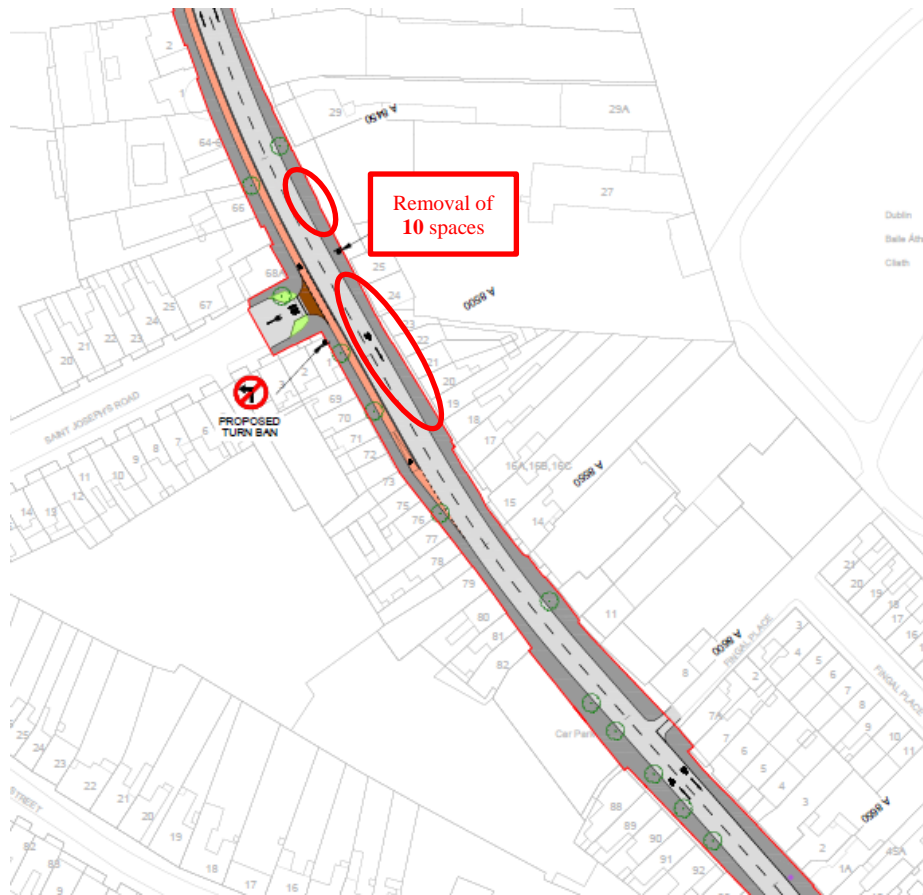


Figure 3: Proposed scheme design on Prussia Street

There are a number of retail and commercial premises along Prussia Street which have off-street parking. Limited stay and resident parking will continue to be available on St Joseph's Road (where there are spaces for approximately 35 cars). Overall, it is anticipated that more efficient use of off-street parking supply and continued use of on-street parking on local side roads will provide a robust and adequate parking provision into the future – when considering also the much improved accessibility by bus and cycle modes.

Manor Street and Stoneybatter

On Manor Street there is presently kerbside parking with space for approximately 71 cars (including 2 disabled bays), available for Pay & Display and Permit Parking from 07.00 to 19.00 Monday to Saturday. With the Proposed Scheme in place, changes are illustrated in **Figure 4** and **Figure 5**.

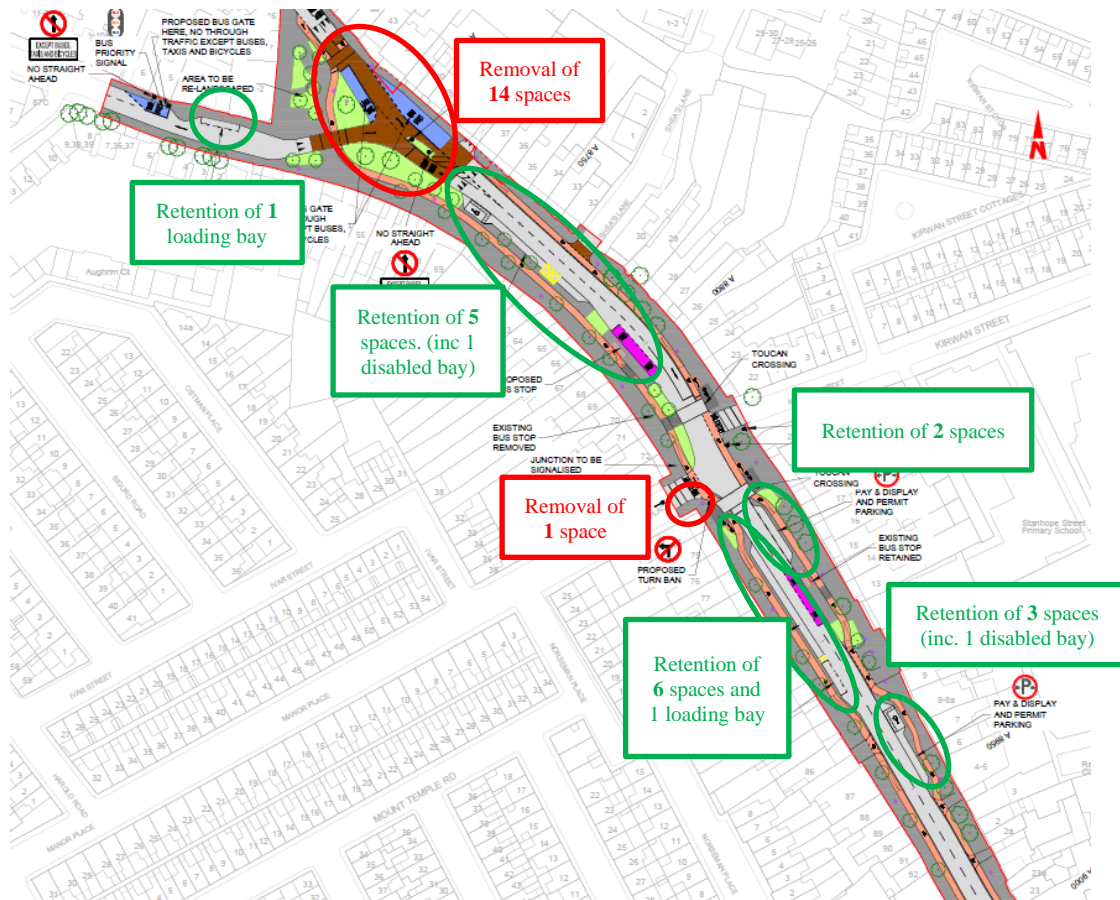


Figure 4: Proposed scheme design on Manor Street



Figure 5: Proposed scheme design on Stoneybatter

A summary of the changes on Aughrim Street, Manor Street and Stoneybatter are as follows:

- The Proposed Scheme includes removal of 11 spaces outside Kavanagh’s Lounge Bar and Aughrim Street – an area which is planned for a major public realm upgrade. The continued presence of Pay & Display parking on side streets such as Aughrim Street will ensure that, in future, parking will still be available locally.
- On Manor Street, it is intended to reduce parking supply from 58 spaces to 18 spaces (including 2 disabled bays), with those bays located in protected laybys

which should improve parking turnover, which together with continued occasional use of Pay & Display parking on local side streets (Kirwan Street and Manor Place), will ensure that future parking provision will be adequate to serve local needs. One space will also be removed on Manor Place.

- On Manor Street's southern section and along Stoneybatter, alongside the northbound lane, there is currently space along the kerb to allow vehicles to park for part of the day only, located directly along an advisory cycle lane (northbound). These spaces are only theoretically available from 10.00 to midday (due to the Clearway regulation from 07.00 to 10.00 and 12.00 to 19.00) – and hence their removal to allow for provision of a northbound off-road cycle track will have minimal impact on overall parking supply, and hence has not been included in the assessment of parking impact.

A summary of the changes to parking provision on the scheme corridor is shown in **Table 5**.

Table 5: Existing and Proposed Parking and Supply Summary (Prussia Street / Manor Street / Stoneybatter)

Sub-section	Parking Facilities	Existing	Proposed	Loss of Parking
Prussia Street	Designated Paid Parking and Permit Parking (from 10am)	10 spaces	0 spaces	10 spaces
Manor Place	Designated Paid Parking and Permit Parking	1 space	0 space	1 space
Manor Street / Stoneybatter	Designated Paid Parking and Permit Parking	58 spaces	16 spaces	42 spaces
	Disabled Permit Parking	2 spaces	2 spaces	0 spaces
	Loading/Unloading in designated loading bays	3 bays (4 spaces)	2 bays (5 spaces)	Gain of 1 space
Aughrim Street / Manor Street junction	Designated Paid Parking and Permit Parking	11 spaces	0 spaces	11 spaces
	Loading/Unloading in designated loading bays	1 bay (2 spaces) immediately west Manor St / Aughrim St junction	1 bay (2 spaces)	0 bay

4.2.2 Loading Bays

There are presently 4 separate loading bays, 3 of which are on Manor Street and Stoneybatter, and another on the southern end of Aughrim Street at the junction with Manor Street. With the Proposed Scheme infrastructure in place, loading bay proposals are as follows:

- On Stoneybatter (outside Centra on the eastern kerb line) a loading bay will be retained within a protected kerbed bay, with sufficient space for three or four light vans or a larger lorry. This will serve the local retail premises – with loading activities also likely to be available in adjacent Pay & Display bays and during early morning and late-night periods. Two ‘one vehicle’ loading bays on the western kerbside are proposed to be removed to enable provision of a northbound cycle track, with a new loading bay with space for two small vans or one larger van on the western kerbside located further north on Manor Street.
- At Aughrim Street, the remodelled junction with Manor Street / Prussia Street will retain the existing loading bay which will service local commercial premises including Kavanagh’s Pub.

4.3 Overall Parking Impact

A summary of the parking impact with respect to the change in overall parking supply on the Proposed Scheme corridor at Aughrim St, Prussia Street, Manor Street, Stoneybatter and Manor Place is shown in **Table 6**, which includes consideration of spaces on adjacent streets within an approximate 200m distance.

Table 6: Impact of Parking and Loading Changes for Prussia Street / Manor Street / Stoneybatter / Aughrim Street

Location	Type of Parking		No. of Spaces		
			Baseline	Scheme	Change
Between Brunswick St. North and Aughrim Lane	Disabled	Disabled	2	2	0
Between Brunswick St. North and Aughrim Lane	Loading Bays	Commercial	4	5	+1
Between Brunswick St. North and Aughrim Lane	Designated Paid / Permit	General / Residential	58	16	-42
Near Kavanagh's Pub and on Aughrim St.	Designated Paid / Permit	General / Residential	11	0	-11
Aughrim St.	Loading Bays	Commercial	2	2	0
Manor Place	Designated Paid / Permit	General / Residential	1	0	-1
Between Manor St to NCR	Designated Paid / Permit	General / Residential	10	0	-10
Approx. adjacent on-street parking within 200m			249	249	0
Totals			337	274	-63

5 Parking Impact on Blackhall Place and adjacent streets

5.1 Baseline Parking and Loading Analysis

5.1.1 Corridor On-Street Parking Bays and Regulation

These parking spaces can be used by residents with parking permits and businesses/visitors are able to pay for parking at the ticket machines or online. This section is within the Parking Tariff High Demand Red Zone, which has a rate of €2.70 per hour. The maximum allowable parking time at Pay & Display areas is three hours and vehicles are not permitted to return to the same street until one hour has elapsed.

The location and provision of current spaces (or bays) on Blackhall Place, Blackhall Street, Queen Street, and Brunswick Street North is illustrated in **Appendix A**.

The current spaces are also summarised as follows:

- Parking on the northbound bus lane on Blackhall Place between Benburb Street and Hendrick Place for Pay & Display and Permit Parking from 07:00 to 16:00 Monday to Friday and 07:00 to 19:00 on Saturday, with sufficient space for approximately 4 cars. This section is a bus lane from Monday to Friday between 16.00 and 19.00.
- Parking on the northbound bus lane on Blackhall Place between Hendrick Place and Blackhall Street (for Pay & Display and Permit Parking from 10.00 to 16.00 Monday to Saturday), with sufficient space for approximately 4 cars. This section is a bus lane from Monday to Saturday 07.00 to 10.00 and 16.00 and 19.00.
- Kerbside parking with space for approximately 18 cars in the southbound direction on Blackhall Place between King Street North and Blackhall Street available for Pay & Display and Permit Parking from 07.00 to 24.00 Monday to Sunday. Blackhall Place has 15 permits allocated for the full street.
- Kerbside parking with space for approximately 6 cars on Brunswick Street North for Pay & Display and Permit Parking (6 permits are allocated for this street) from 07.00 to 19.00 Monday to Saturday.
- On Queen Street Pay & Display and Permit Parking (4 permits are allocated Queen street) for 3 cars available from 10.00 to 16.00 Monday to Friday. On Queen Street there is also informal parking for up to 13 car parking spaces (single yellow-line) available between King Street North and Blackhall Street on the western side of the road between 19:00 and 07:00.
- On King Street North there is also space along the kerb for parking on the northern side of the road . These spaces are only theoretically available from 10.00 to midday (due to the Clearway regulation from 07.00 to 10.00 and 12.00 to 19.00) – and hence their removal will have minimal impact on overall

parking supply, and has not been included in the assessment of parking impact.

- Blackhall Street has designated on-street parallel parking (unregulated) for approximately 19 cars; however, observations indicate that the southern side of Blackhall Street is being used by more cars due to cars parking at an angle to the kerb lanes (instead of parallel) and protruding into the wide traffic lane.

A summary of existing parking in the Blackhall Place area is summarised in **Table 7**.

Table 7: Existing Parking and Loading Spaces at Blackhall Place area

Sub-section	Existing Parking Facilities	Number of Spaces
Blackhall Place	Designated Paid Parking and Permit Parking	26 spaces
Queen Street	Designated Paid Parking and Permit Parking	3 spaces
Brunswick Street North	Designated Paid Parking and Permit Parking	6 spaces
	Loading Bays	1 bay
Blackhall Street	Designated Parking (unregulated)	19 spaces
Blackhall Street	Loading Bays	1 bay

5.1.2 Loading Activity

There are no loading bays on Blackhall Place; however, there is a loading bay on Brunswick Street North near its junction with George's Lane. There is also a loading bay located on Blackhall Street next to its junction with Blackhall Place.

It should be noted that there are also loading bays available on the local side streets such as Hendrick Street and Benburb Street.

5.1.3 On-Street Parking Bays and Regulation on Local Side Streets

There is a network of side streets adjacent to the Proposed Scheme, with parking arrangements as follows:

- On Benburb Street (east of Blackhall Place), there is space for approximately 13 cars (at Pay & Display and Permit Parking spaces, from 10.00 to 16.00 Monday to Saturday);

- On Benburb Street (immediately west of Blackhall Place), there is space for parking available; however, this is a Clearway between 07.00 and 10.00 and 16.00 and 19.00 Monday to Saturday;
- On Hendrick Street, (east of Blackhall Place), there is space for approximately 8 cars at Pay & Display and Permit Parking (15 permits are allocated to this street) from 07.00 to 19.00 Monday to Saturday;
- On Hendrick Place (west of Blackhall Place), there is space for approximately 9 cars (at Pay & Display and Permit Parking spaces, from 07.00 to 24.00 Monday to Saturday);
- Oxmantown Lane is a narrow street; however, there are informal parking spaces utilised in this area according to observation;
- On Blackhall Green, there are informal parking spaces for approximately 10 cars; however, this is regulated by a sign for private property and notes that enforcement includes clamping of unauthorised vehicles;
- On Haymarket, there are Pay & Display and Permit Parking between 07.00 and 19.00 Monday to Saturday available for approximately 47 spaces (including 1 disabled parking bay); and
- On Smithfield, there are Pay & Display and Permit Parking between 07.00 and 19.00 Monday to Saturday available for approximately 18 spaces.

These side streets add more than 110 parking spaces to the overall parking supply – which are within easy walking distance of Blackhall Place. From observations, these spaces are relatively busy but with some spaces available during the day (on a typical weekday).

5.1.4 Land Use

Blackhall Place is surrounded by a mixture of land uses that consist of commercial, education, retail and residential and is shown in **Appendix B**.

The western side of Blackhall Place is predominantly occupied by the headquarters of the Law Society of Ireland. It has parking within its premises and is accessible via Blackhall Place. The rest of the western side are mixed use with mostly residential (including student accommodation) and commercial properties.

The western side of the corridor is predominantly residential with small shops and restaurants towards Brunswick Street North.

As a majority of the properties are residential, parking demand may be low during the day, which balances with bus lanes being used in this section during this period.

5.2 Proposed Scheme Parking Proposals

5.2.1 Proposed On-Street Parking and Loading Bay Supply

With the Proposed Scheme infrastructure in place, there is an associated need to remove some parking spaces to provide improved facilities for pedestrians, cyclists, and buses – which inevitably requires some reallocation of parking road space. This section discusses the planned reduction of on-street parking supply. These are illustrated in **Figure 6** and **Figure 7** to show the proposed future on-street parking provision on the bus corridor.

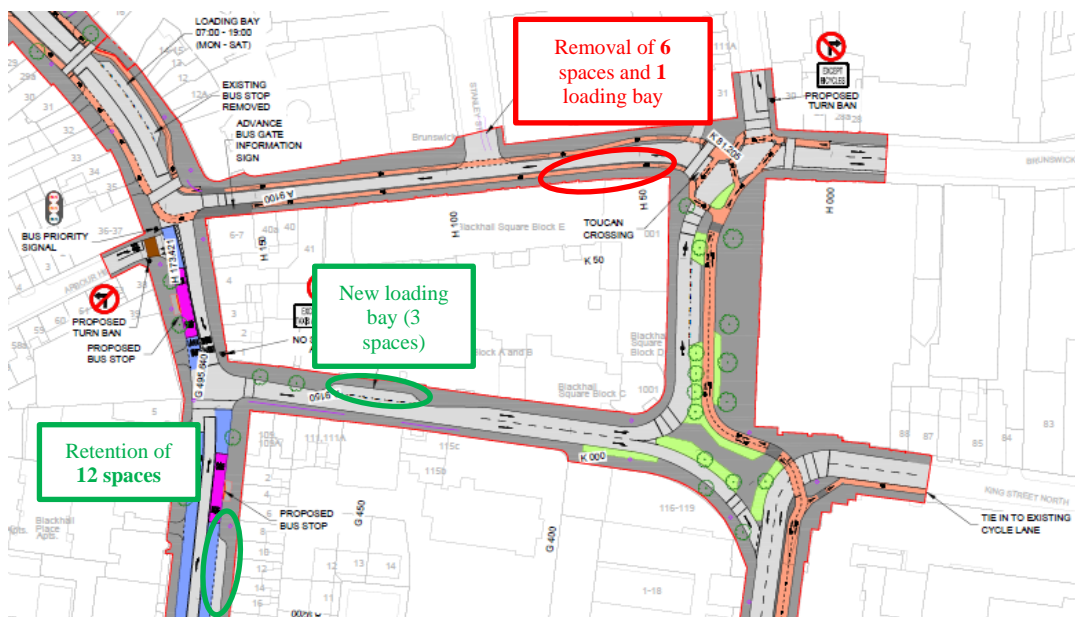


Figure 6: Proposed scheme design parking changes for Brunswick Street North, King Street North and the northern section of Blackhall Place

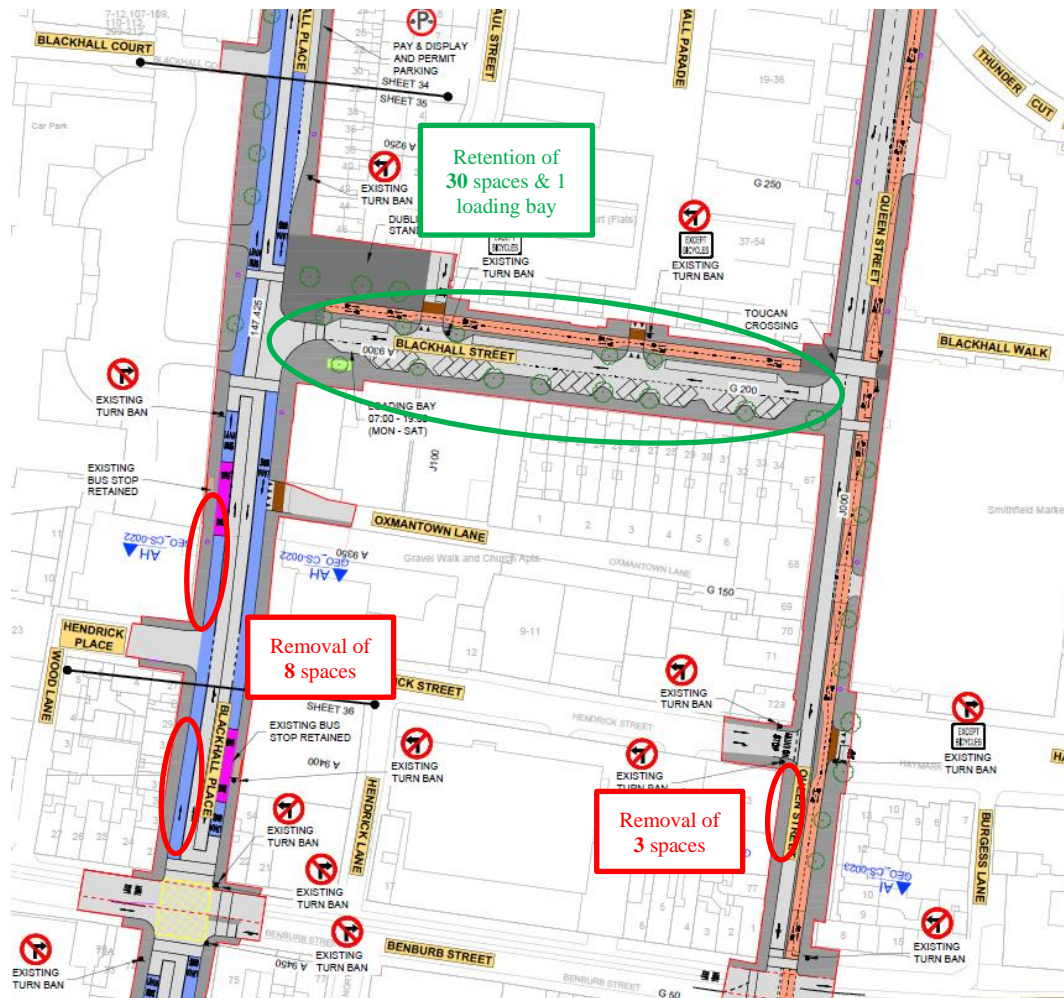


Figure 7: Proposed scheme design parking changes for Queen Street, Blackhall Street, and the southern section of Blackhall Place

Overall, it is anticipated that more efficient use of off-street parking supply and continued use of on-street parking on local side roads will provide a robust and adequate parking provision into the future. On Blackhall Place, all parking bays located on the proposed bus lanes will be removed, with the exception of kerbside parking on the eastern side of the street between King Street North and Blackhall Street, which will be reduced from 18 to 12 spaces. The continued availability of Pay & Display parking on side streets such as Benburb Street, Hendrick Place, Hendrick Street and Blackhall Street will ensure that future parking provision will be adequate to serve local needs.

Brunswick Street North is proposed to be a one-way street with cycle tracks in both directions, meaning that 3 parking spaces and 1 loading bay in this area are proposed to be removed. However, a new loading bay is proposed for King Street North, which will offset the loss of the loading bay on Brunswick Street North.

A summary of changes to parking provision on the scheme corridor is shown in **Table 8**.

Table 8: Existing and Proposed Parking and Loading Supply Summary (Blackhall Place and adjacent streets)

Sub-section	Parking Facilities	Existing	Proposed	Change in Parking
Blackhall Place	Designated Paid Parking and Permit Parking	26 spaces	12 spaces	Reduction of 14 spaces
Queen Street	Designated Paid Parking and Permit Parking	3 spaces	0 spaces	Reduction of 3 spaces
King Street North	Loading Bays	0 spaces	1 space (3 spaces)	Additional loading bay (3 spaces)
Brunswick Street North	Designated Paid Parking and Permit Parking	6 spaces	0 spaces ¹	Reduction of 6 spaces
	Loading Bays	1 bay (2 spaces)	0 bay	Reduction of 1 bay (2 spaces)
Blackhall Street	Designated Parking (unregulated)	19 spaces	30 spaces	Additional 11 spaces
Blackhall Street	Loading Bays	1 bay (1 space)	1 bay (1 space)	Retention of 1 bay (1 space)

5.2.2 Loading Bays

The loading bay proposed to be removed on Brunswick Street North, will be adequately replaced, in respect of local deliveries, by the proposed new loading bay on King Street North. The loading bay on Blackhall Street will be retained.

5.3 Overall Parking Impact

A summary of the parking impact with respect to the change in overall parking supply on the section of the Proposed Scheme corridor between Brunswick Street North and Ellis Quay is shown in **Table 9**, which includes consideration of spaces on adjacent streets within an approximate 200m distance.

Table 9: Impact of Parking and Loading Changes for Blackhall Place and adjacent streets

Location	Type of Parking		No. of Spaces		
			Baseline	Scheme	Change
Blackhall St	Designated unregulated	General / Residential	19	30	+11
Blackhall St	Loading Bays	Commercial	1	1	0
Between Arran Quay and King St North	Designated Paid / Permit	General / Residential	3	0	-3
Between King St North and Benburb St.	Designated Paid / Permit	General / Residential	26	12	-14
Between Blackhall Place and Queen St	Loading Bays	Commercial	0	3	3
Brunswick St North	Designated Paid / Permit	General / Residential	6	0	-6
Brunswick St North	Loading Bays	Commercial	2	0	-2
Approx. adjacent on-street parking within 200m			118	118	0
Totals			175	164	-11

6 Summary of Parking Impact

6.1 Summary of Parking Changes

With the Proposed Scheme infrastructure in place, there is an associated need to remove some parking spaces to provide improved facilities for pedestrians, cyclists, and buses – which inevitably requires some reallocation of on-street parking space. The proposed changes in parking and loading provision along the Proposed Scheme are summarised in **Table 10** and **Table 11**.

Table 10: Summary of Parking Changes (Blanchardstown to City Centre Scheme)

Location	Baseline		Proposed		Change
	Corridor	Adjacent	Corridor	Adjacent	
Navan Road section (Ashtown Road to Old Cabra Road)	24	24	4	24	-20
Prussia St / Manor St / Stoneybatter / Aughrim St section	82	249	18	249	-64
Blackhall Place section	54	118	42	118	-12

Table 11: Summary of Loading Changes (Blanchardstown to City Centre Scheme)

Location	Loading Bays		Change
	Baseline	Proposed	
Navan Road section (Ashtown Road to Old Cabra Road)	0	0	0
Prussia St / Manor St / Stoneybatter / Aughrim St section	6	7	+1
Blackhall Place section	3	4	+1

6.2 Summary of Parking Impact

With the Proposed Scheme infrastructure in place, the impacts of the change on on-street parking are as follows:

- **Navan Road:** The change in parking involves removal of approximately 19 informal spaces, and retention of approximately 4 of an existing 5 space layby (on Navan Road between Ashtown Road and Nephin Road). Although these spaces are proposed to be removed, spaces on adjacent side streets will still be available. It is also noted that managed use of private off-street parking at, for example, local churches and schools will ensure that visitors to these facilities will be able to park as necessary, and as most houses on the scheme corridor have driveways, residents will be able to utilise their available off-road space for parking (rather than seek to park on-street).

Prussia Street / Manor Street / Stoneybatter: The parking changes of the Proposed Scheme involves removal of all 10 parking spaces (currently only available from 10am onwards during the day) from Prussia Street. It is noted however that on-street parking on adjacent side streets will continue to provide a parking supply for local use.

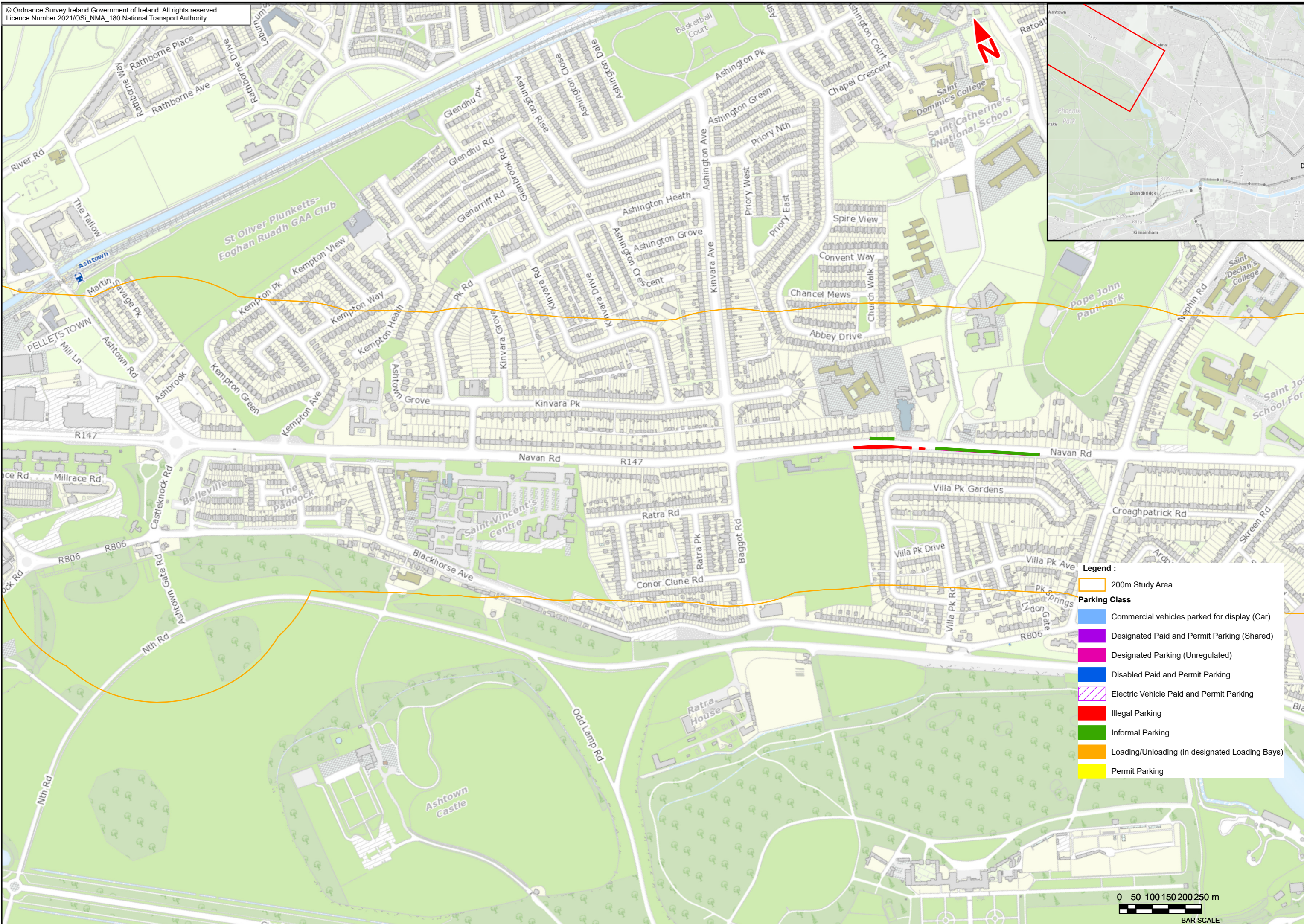
On Manor Street, protected bays will be provided for 25 spaces as part of the Proposed Scheme, which equates to a reduction of 63 spaces for Manor Street, Stoneybatter, Manor Place and Aughrim Street. It is also noted that the adjacent side streets will continue to provide a local parking supply of over 200 spaces close-by. The proposed scheme has loading bays at a similar provision to what is currently available.

Blackhall Place and adjacent streets: On the Blackhall Place section, the Proposed Scheme involves provision of 46 spaces, a reduction of 11 from the present situation. Adjacent streets will continue to provide a local parking supply of over 100 spaces. Loading bays will be provided at a similar level to what is available at present.

Appendix A

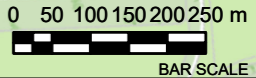
Existing On-Street Parking Spaces on Blanchardstown to City Centre Scheme

A1 Existing On-Street Parking Spaces Navan Road (between Ashtown Road and Nephin Road)

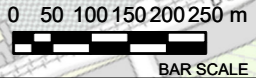
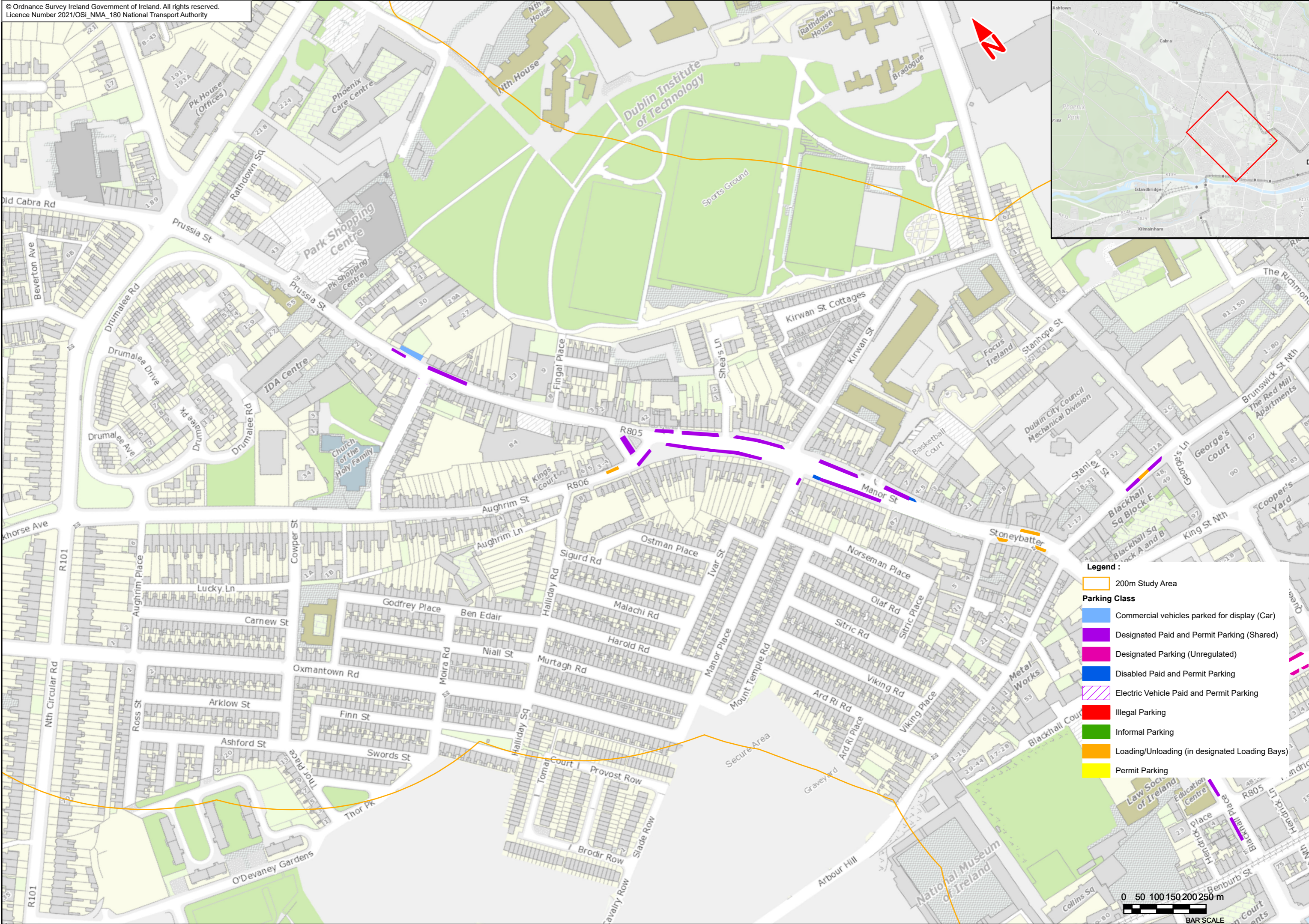


Legend :

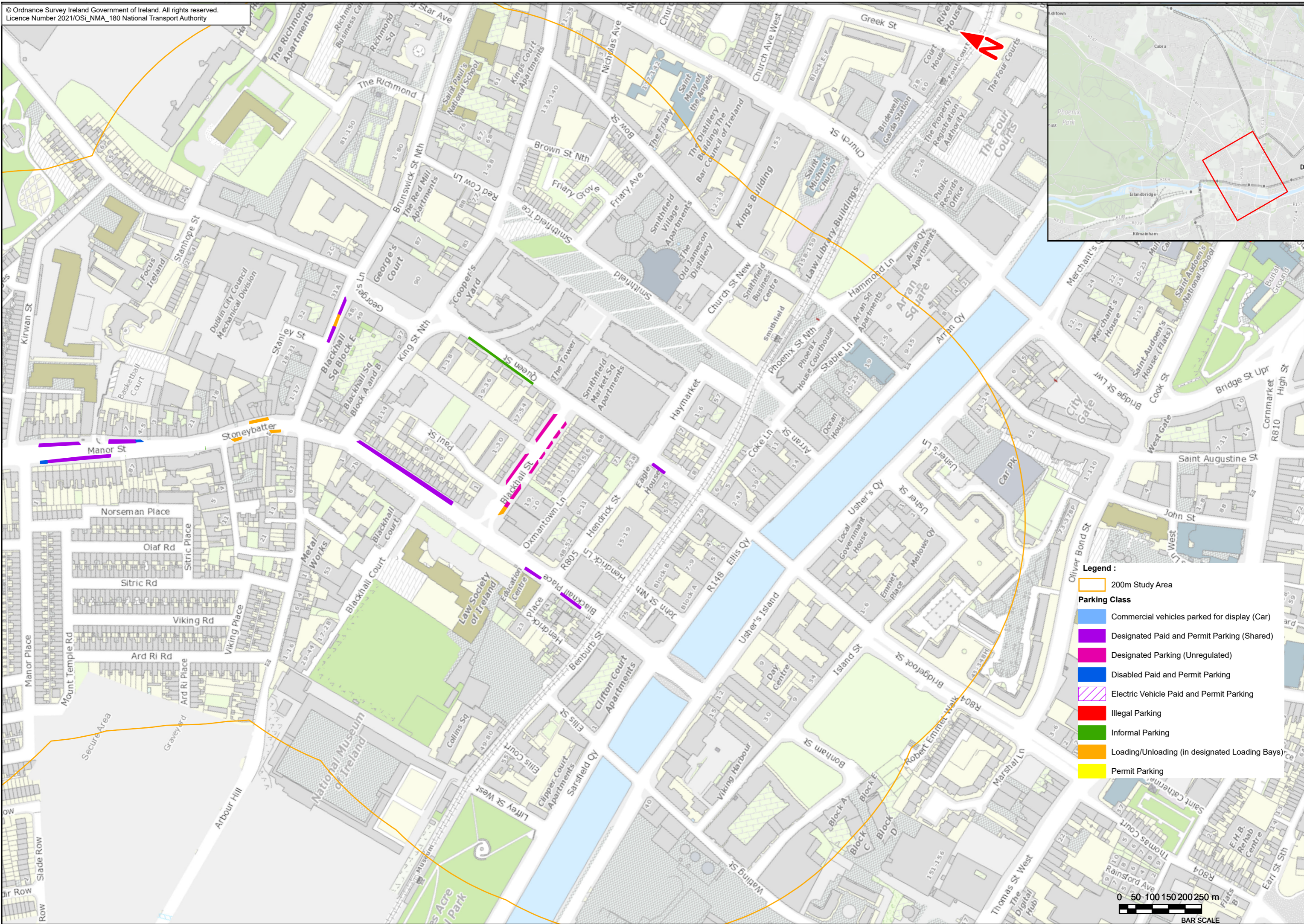
-  200m Study Area
- Parking Class**
-  Commercial vehicles parked for display (Car)
-  Designated Paid and Permit Parking (Shared)
-  Designated Parking (Unregulated)
-  Disabled Paid and Permit Parking
-  Electric Vehicle Paid and Permit Parking
-  Illegal Parking
-  Informal Parking
-  Loading/Unloading (in designated Loading Bays)
-  Permit Parking



A2 Existing On-Street Parking Spaces Prussia Street, Manor Street, Stoneybatter and Aghrim Street (between North Circular Road and Brunswick Street North)

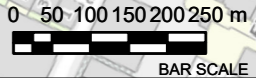


A3 Existing On-Street Parking Spaces (Blackhall Place, Queen Street, Brunswick Street North, King Street North and Blackhall Street)



Legend :

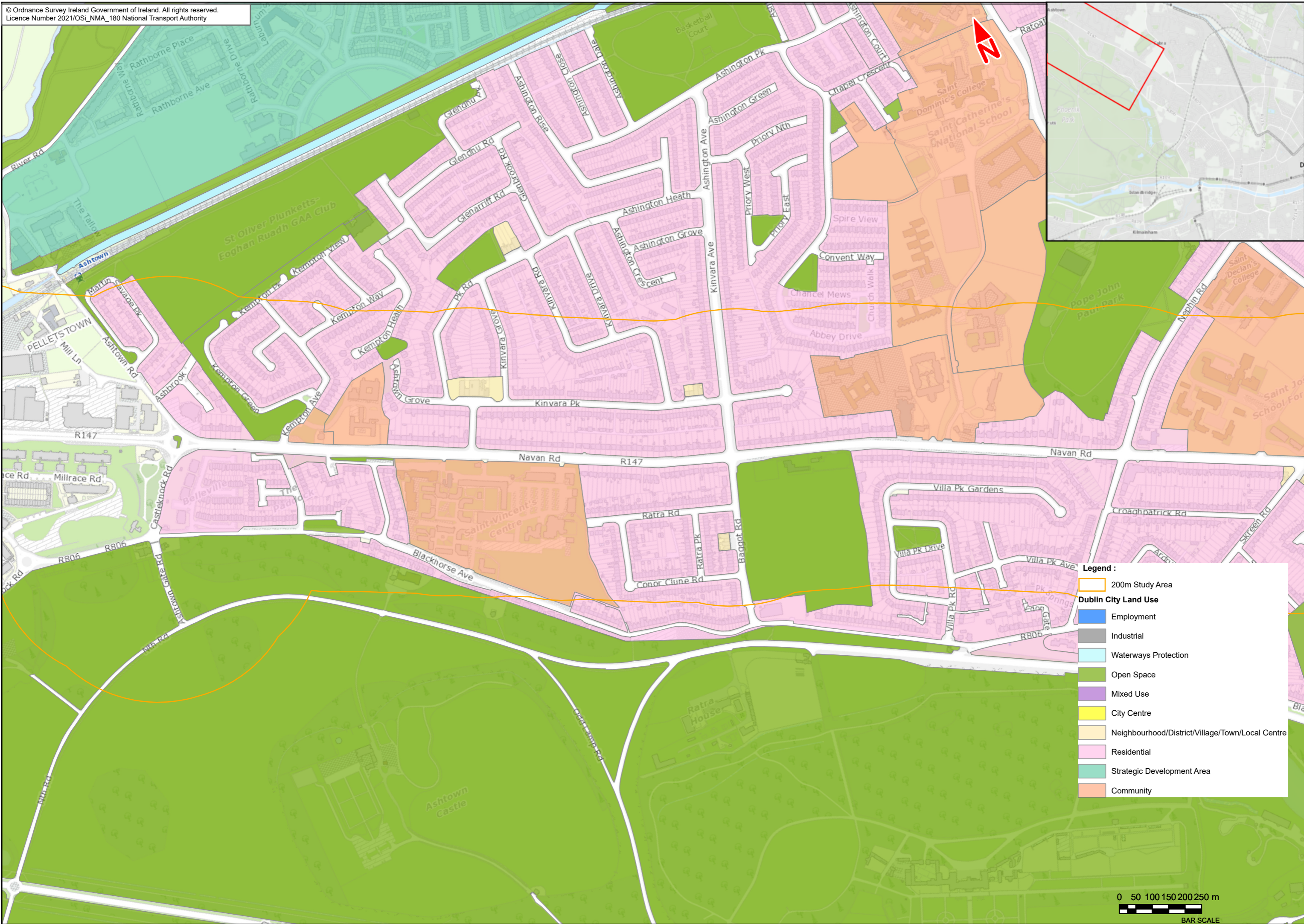
- 200m Study Area
- Parking Class**
- Commercial vehicles parked for display (Car)
- Designated Paid and Permit Parking (Shared)
- Designated Parking (Unregulated)
- Disabled Paid and Permit Parking
- Electric Vehicle Paid and Permit Parking
- Illegal Parking
- Informal Parking
- Loading/Unloading (in designated Loading Bays)
- Permit Parking



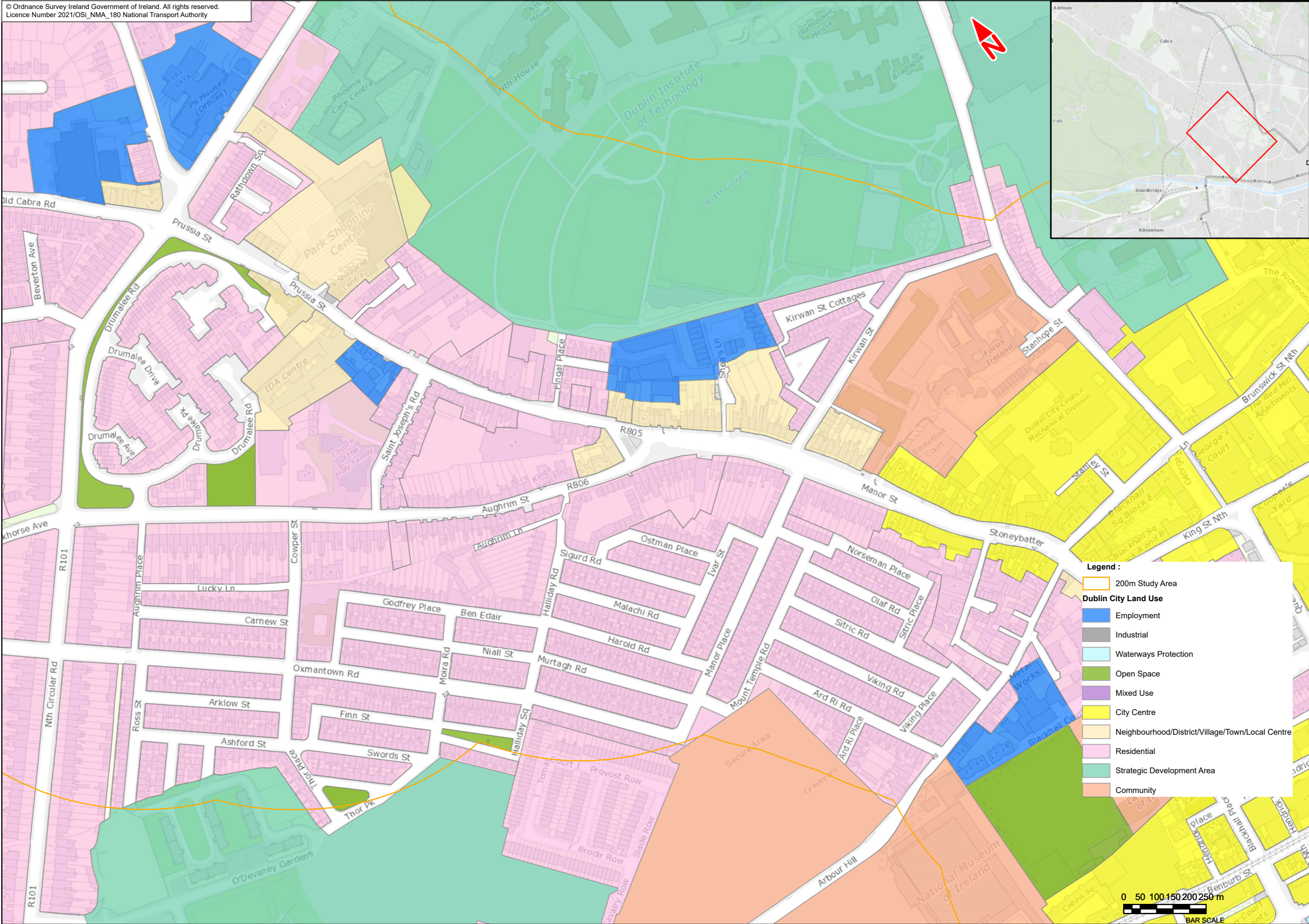
Appendix B

Existing Land Use on Blanchardstown to City Centre Scheme Parking Study Area

B1 Existing Land Use Navan Road (between Ashtown Road and Nephin Road)

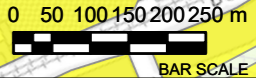


B2 Existing Land Use Prussia Street, Manor Street, Stoneybatter and Aughrim Street (between North Circular Road and Brunswick Street North)

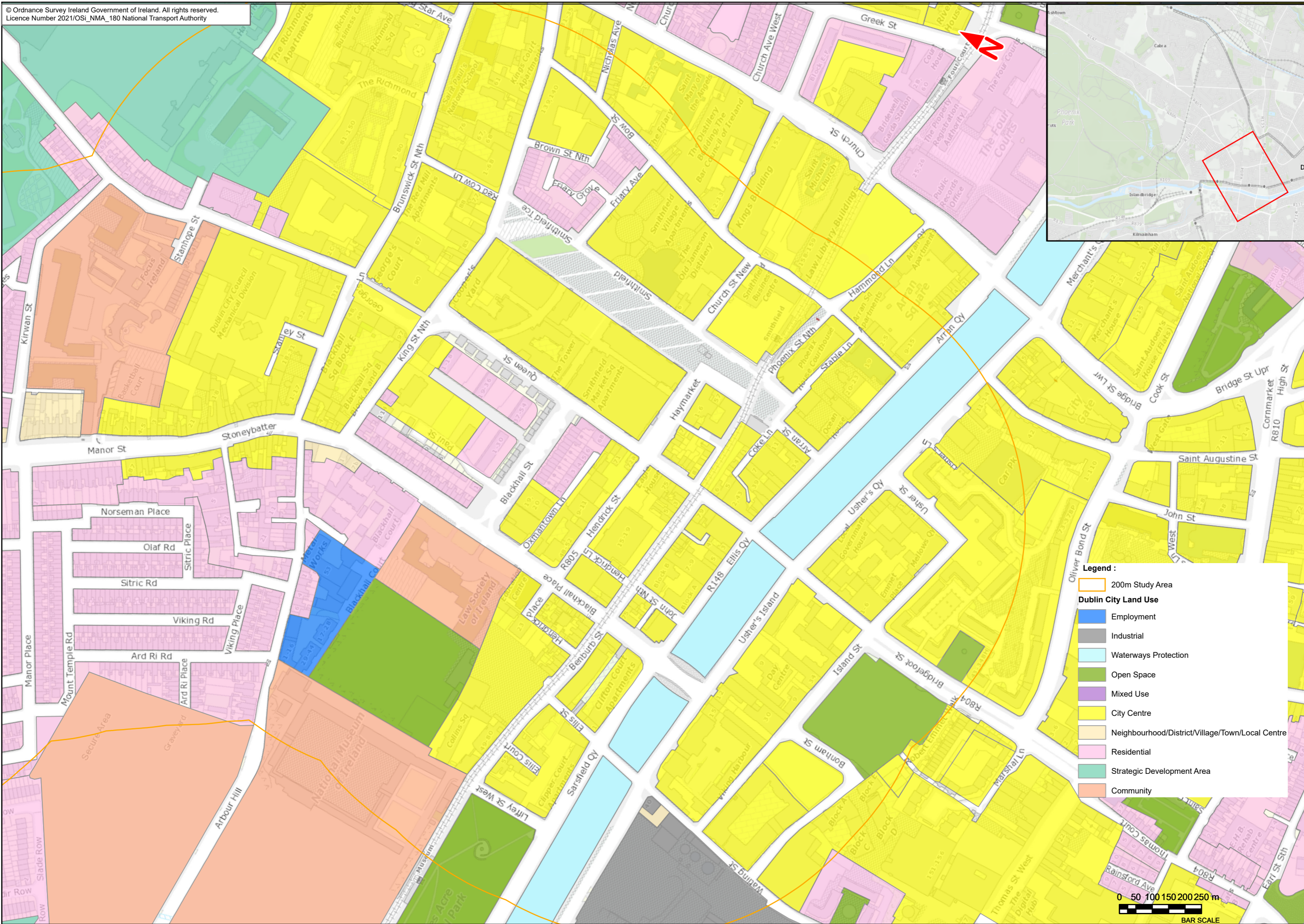


Legend :

- 200m Study Area
- Dublin City Land Use**
 - Employment
 - Industrial
 - Waterways Protection
 - Open Space
 - Mixed Use
 - City Centre
 - Neighbourhood/District/Village/Town/Local Centre
 - Residential
 - Strategic Development Area
 - Community

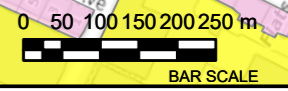


B3 Existing Land Use (Blackhall Place, Queen Street, Brunswick Street North, King Street North and Blackhall Street)



Legend :

- 200m Study Area
- Dublin City Land Use**
- Employment
- Industrial
- Waterways Protection
- Open Space
- Mixed Use
- City Centre
- Neighbourhood/District/Village/Town/Local Centre
- Residential
- Strategic Development Area
- Community



BAR SCALE